

Revised Plan Sheet No. 1
IN A.C. 112
R 2 8/1/33

FISCAL YEAR	JOB NO.	SHEET NO.	TOTAL SHEETS	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
1931	3161	1	9	6	ARK.	E-274-B	1931	1	9

Revised 8-9-33

INDEX OF SHEETS

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2	3208	Layout of Bridge No. 1029
3	3209	Details of Piers for Bridge No. 1029
4	2273	Details of Std 30'-0" R.C.D. Gir. Span, 4 Girder Type, 4 Pile Bent, 20'-0" Clear Roadway.
5	2419	Details of Std 110'-0" Low Truss Span, 20'-0" Clear Roadway.
6	3210	Partial Layout of Bridge No. 1623
7	3211	Partial Layout of Bridge No. 1623 and Layout of Bridge No. 1622
8	E.S.1	Typical Section of Earth Approaches
9	F.P. 2	Federal Aid Project Markers

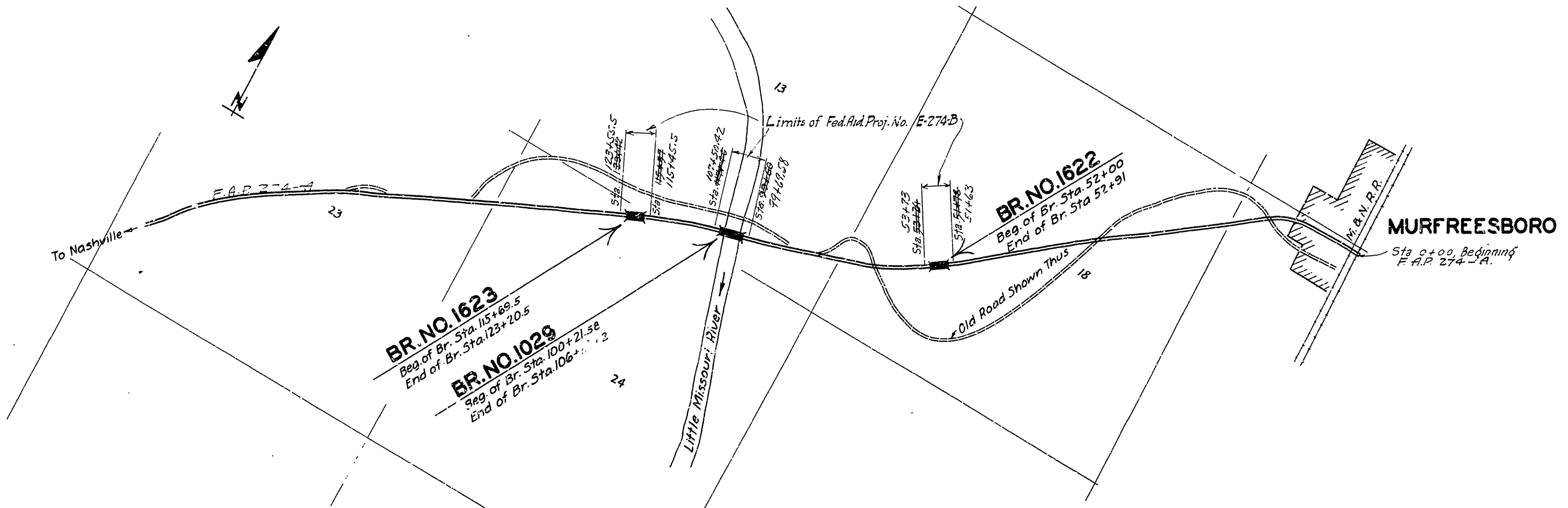
STATE OF ARKANSAS
STATE HIGHWAY COMMISSION
PLAN OF PROPOSED BRIDGES
ON
MURFREESBORO-NASHVILLE ROAD
PIKE COUNTY

ROUTE 27 SEC. 4
JOB No 3161
FEDERAL AID PROJECT No E-274-B

QUANTITIES

Item No.	SP	1/2	Earth Excavation	5400	Cu. Yds.
"	"	13	Dry Excavation for Structures	457	Cu. Yds.
"	"	13	Wet Excavation for Structures	816	Cu. Yds.
"	"	13	Solid Rock Excavation for Structures	100	Cu. Yds.
"	"	54	Class "A" Concrete	42671	Cu. Yds.
"	"	54	Class "B" Concrete	153760	Cu. Yds.
"	"	55	Reinforcing Steel	420996	Lbs.
"	"	56	Structural Steel (Truss Bridges)	315100	Lbs.
"	"	SR 68	Precast Concrete Piling	4016	Lin. ft.
"	"	SR 69	Rip Rap 1 foot thick	450	Cu. Yds.
"	"	74	Concrete Paving for Structures	2420	Lin. ft.
"	"	SP	Removal of Existing Structures	Lump Sum	

Fed. Aid bronze markers of approved design are to be furnished by the State. One project marker will be placed by the contractor at each of the following stations: to wit: 51+46.5, 52+24.92, 99+69.58, 107+50.42, 115+55, 123+55.5, 51+63, 53+73



LAYOUT
Scale: 1" = 1000'

LENGTH OF F.A.P. 274-B	=	1800.84	
LENGTH OF BRIDGES	=	1711.0' = 0.324 Miles	0.341 Miles
LENGTH OF EMBANKMENT	=	1508'-10" = 0.285 "	0.285 "
LENGTH OF JOB	=	2021'-3" = 0.468 "	0.468 "

Specifications approved by Chief, Bureau of Public Roads, September 28, 1925, and adopted by State Highway Commission May 30, 1925 with revisions and Special Provisions as follows:

REVISIONS	
Pamphlet A	Issued Sept. 1st, 1928 Approved June 1st, 1929
" B	Issued Nov. 1st, 1929
" C	July 1st, 1930
" D	Revised July 1st, 1928 Approved Nov. 24th, 1928
" E	Revised Jan. 1st, 1930
" F	Revised Aug. 1st, 1929
" G	Revised June 1st, 1929
" H	Revised Jan. 1st, 1930
" I	May 30th, 1925
" J	Revised Jan. 1st, 1929 Approved June 1st, 1925
" K	May 30th, 1925
" L	Revised Jan. 1st, 1929 Approved June 1st, 1929
" M	May 1st, 1931

SPECIAL PROVISIONS	
Item	No. of Sheets
Rip Rap	1
Pamphlet M	51
Earth Excavation	3
Concrete Aggregate	2
Precast Concrete Piling	3
Field Engineers Office	1
Aluminum Paint	1
Section 82	1
Structural Steel	1
Contractor Furnish All Material	1
Removal of Existing Structures	1
Relating to all types of work	4
" " Control of labor	3
" " Classification of labor	3
Revision-amendment to art. 2.7 of spec.	1

APPROVED
CHIEF ENGINEER - U. S. BUREAU OF PUBLIC ROADS
APPROVED
DISTRICT ENGINEER - U. S. BUREAU OF PUBLIC ROADS
APPROVED
CHIEF - U. S. BUREAU OF PUBLIC ROADS
APPROVED
CHAIRMAN STATE HIGHWAY COMMISSION
APPROVED
STATE HIGHWAY ENGINEER

N.B. Laver
BRIDGE ENGINEER

BRIDGES No. 1622, 1623
& 1029

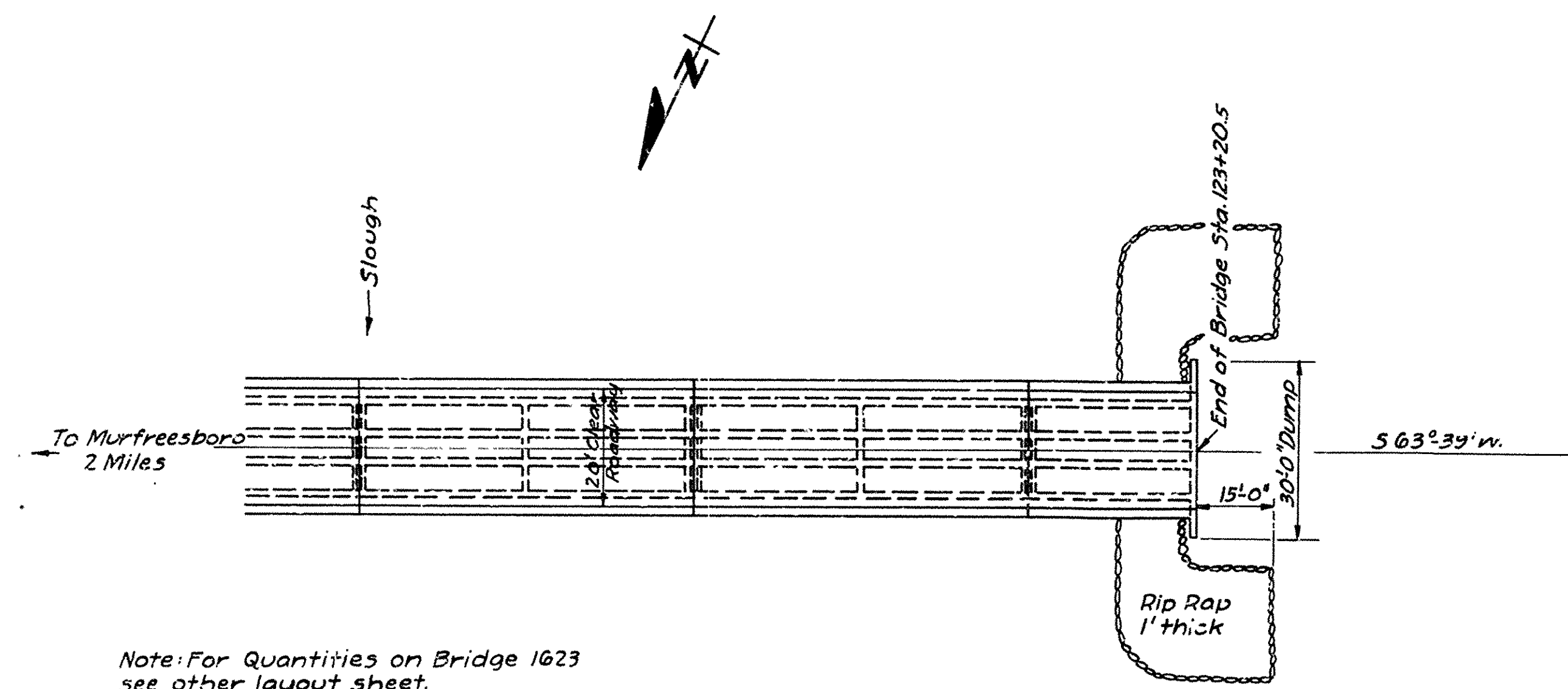
DRAWING No. 3207

FED. AC. D. COSTS	STATE	FED. AID PROJ. No.	FISCAL YEAR	SHEET No.	TOTAL SHEET
	ARK.	E-274-B	1931	7	9

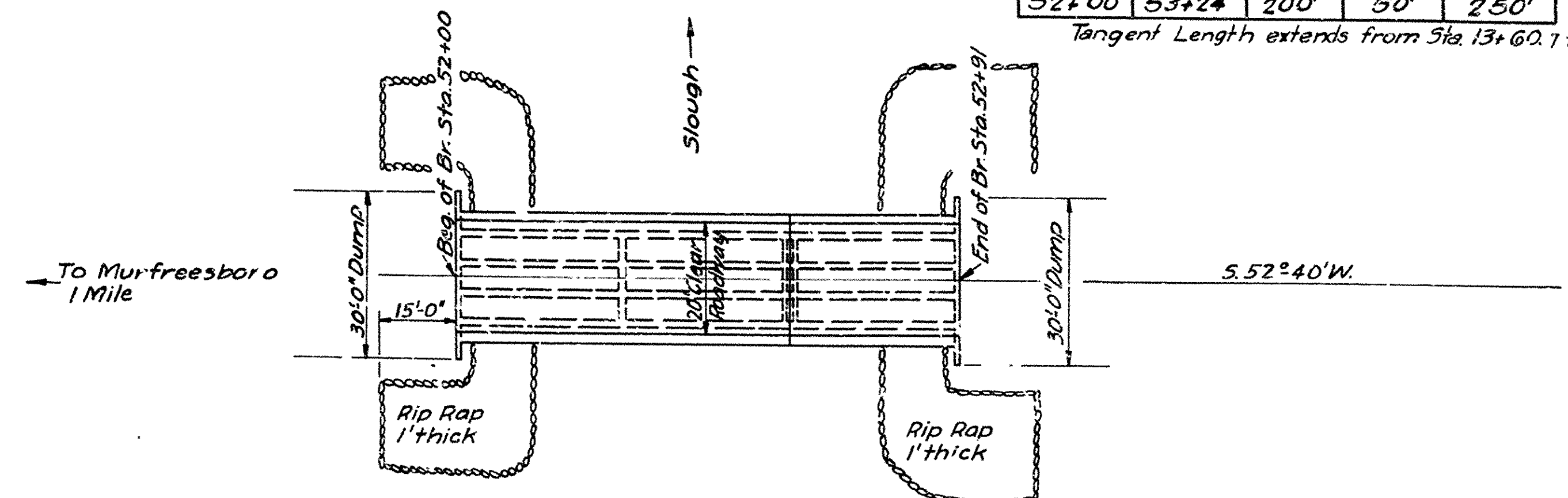
FISCAL YEAR	JOB NO.	SHEET No.	TOTAL SHEETS
1931	3161	7	4

From Sta	To Sta	Rt of E	Lt. of E	Total Width
51+78	52+00	100'	50'	150'
52+00	53+24	200'	50'	250'

Tangent Length extends from Sta. 13+60.7 to Sta. 73+19.4



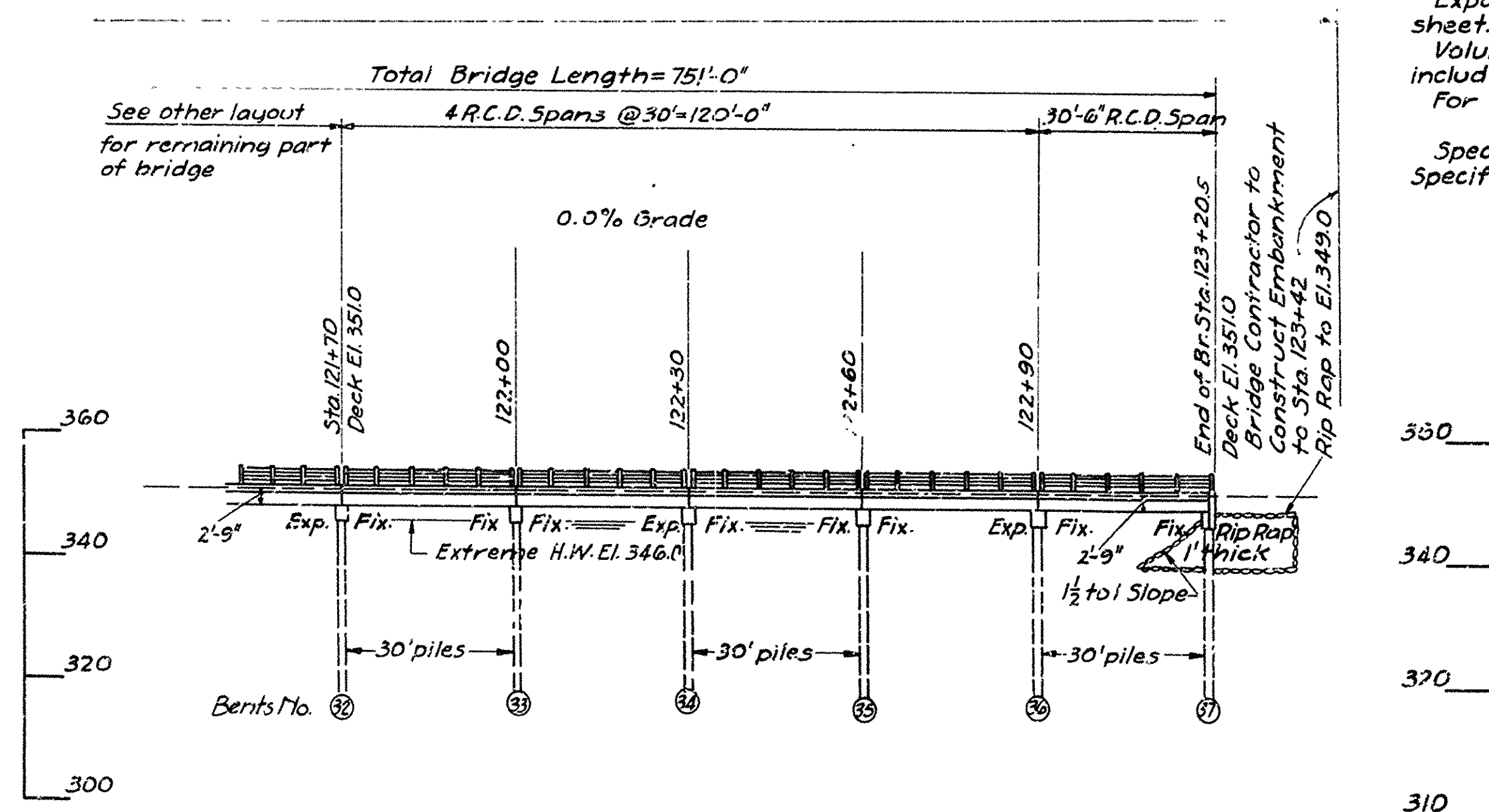
Note: For Quantities on Bridge 1623
see other layout sheet.



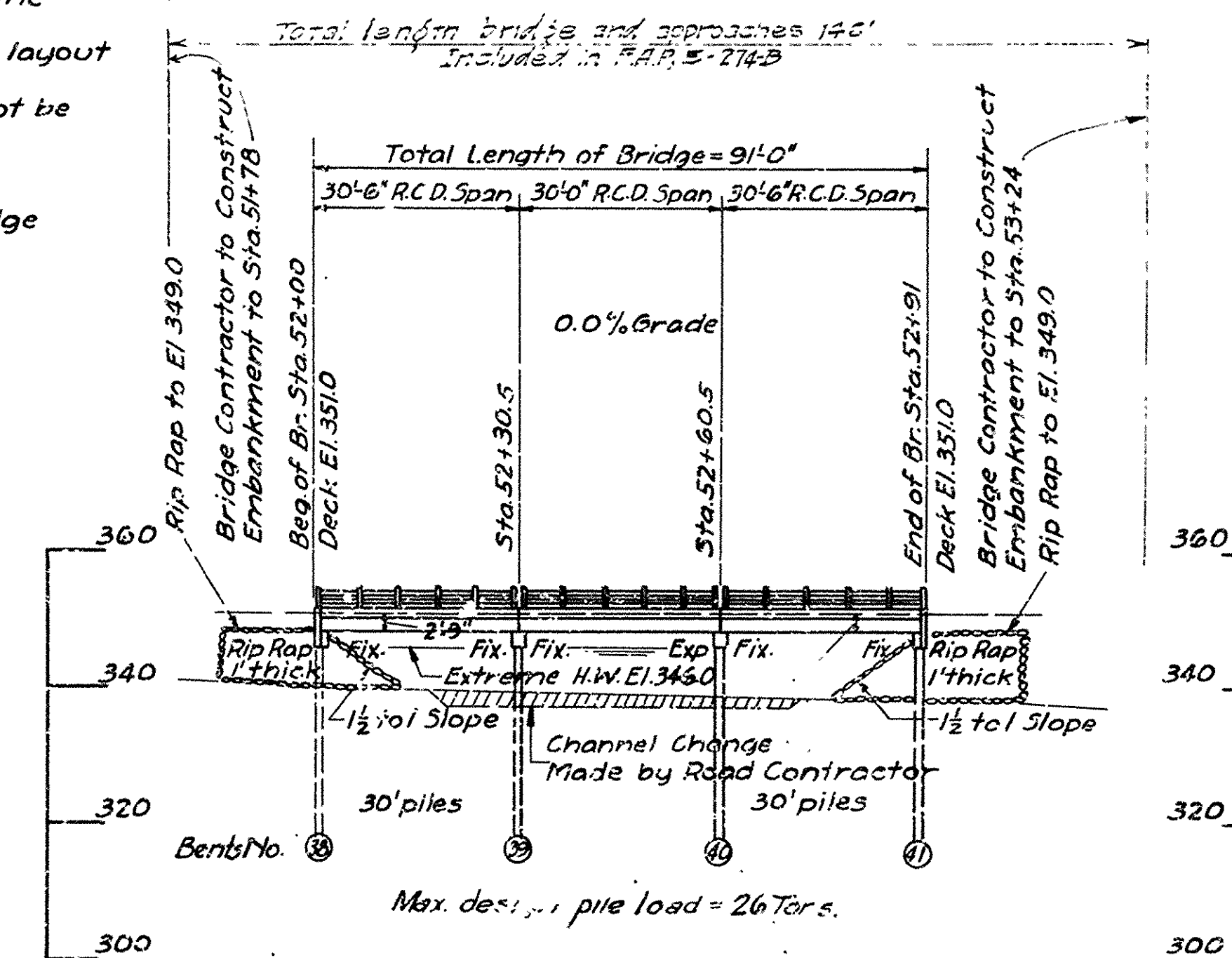
QUANTITIES.			
Item No.	S.P. # 12	Earth Excavation.	1450. Yds.
" "	13	Dry Excavation for Structures.	30. " "
" "	54	Glass "5" Concrete.	1078 " "
" "	55	Reinforcing Steel	2579.5 Lbs.
" "	S.P. # 66.	Precast Conc. Piling.	480. Lin.ft.
" "	S.P. # 69	Rip Rap 1ft thick.	14.0 Cu.Yds.
" "	74.	Concrete Railing for Structures.	200. Lin.ft.
S.P.		Removal of Existing Structure	Lump Sum
"	54	Glass "4" Conc. (2nd gr. marker posts)	0.25 c.y

General Notes
All concrete to be placed in the dry.
For general notes on concrete and piling, see superstructure details.
Lengths of piling shown are assumed for estimating quantities only. Actual lengths to be determined in the field.
Expansion joints to be constructed as shown on layout sheet.
Volume occupied by embedded pile heads will not be included in pay quantities of concrete in caps.
For details, see Drwg No. 2273.

Specifications: Arkansas Standard Road and Bridge Specifications, adopted May 30, 1925 and Revised.



Drainage Area for Bridges 1029, 1622 and 1623 is 400 square miles, mountainous.

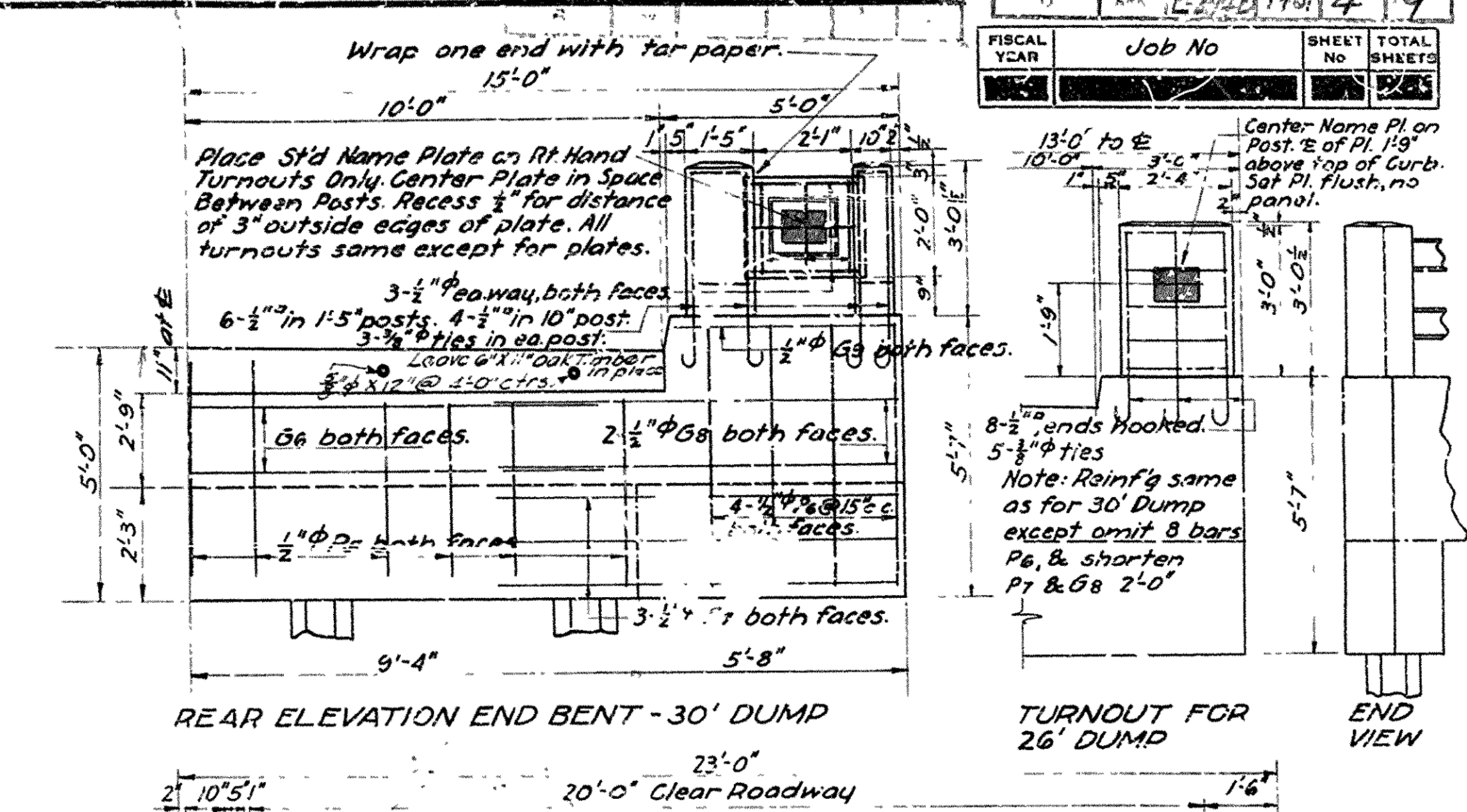


LAYOUTS OF
LITTLE MISSOURI RIVER RELIEF BRS
ON MURFREESBORO-NASHVILLE ROAD
PIKE COUNTY
ROUTE 27 SEC.4

ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

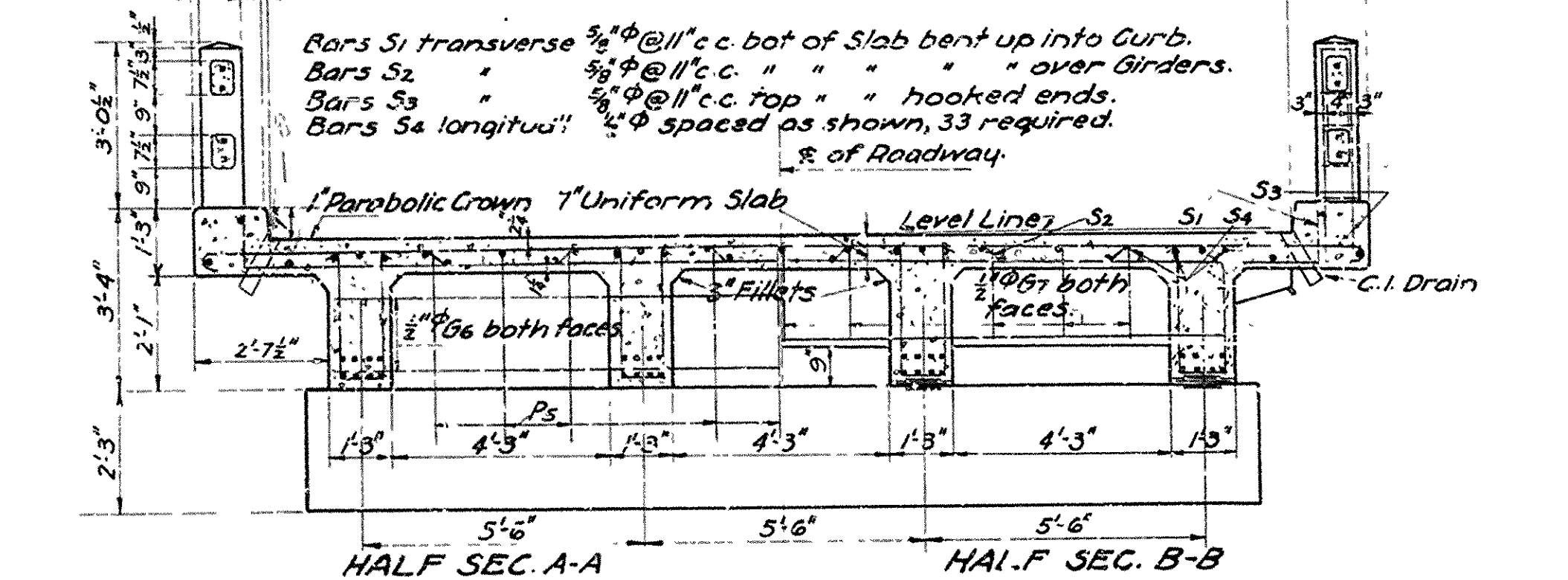
Drawn By: Noe Date: 5-8-31
Traced By: Ja Date: 5-11-31
Checked By: _____ Date: _____
Scale: { 1 in. = 20 ft.
BRIDGE NO. 1622 & 1623 DRAWING NO. 3211

N.B. Garrow
BRIDGE ENGINEER



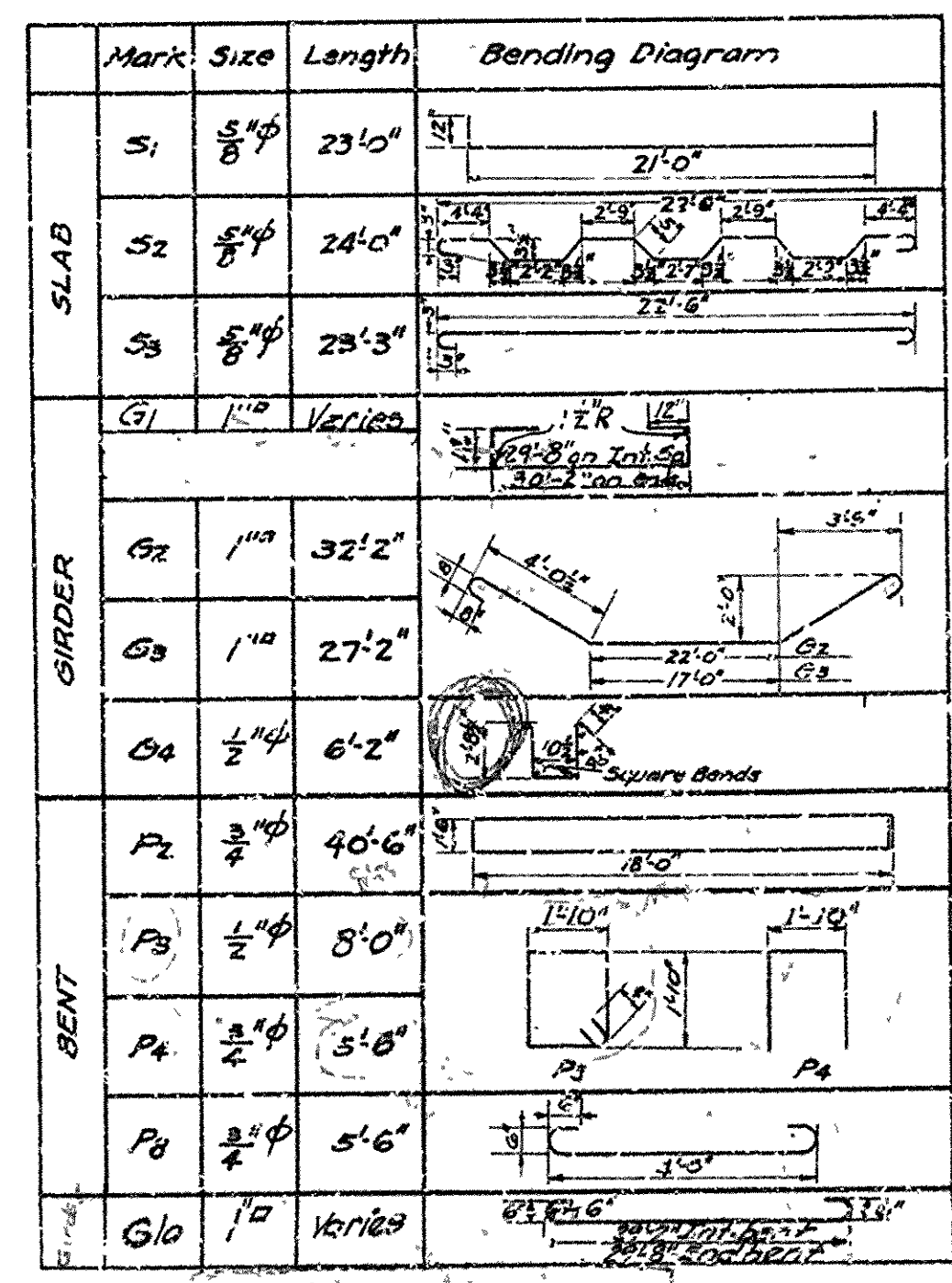
REAR ELEVATION END BENT - 30' DUMP

TURNOUT FOR
26' DUMP



HALF SEC. A-A

HALF SEC. B-B



DETAIL OF CAST IRON DRAIN

12'-0"

3"

7"

3"

1/4" = 1'-0"

DETAIL OF CONC. PILE

PILE DIMENSIONS & REINF'S

L	D	d	P	Vert. Bars	Spiral
1035	16"	8"	8"	8- $\frac{3}{8}$ "	4 Wires
1045	16"	8"	8"	8- $\frac{3}{8}$ "	4 Wires

CRS. holes for 1/4" bolts.

Top Plate 1/2" x 1/2" x 1/2"

BRIDGE ENGINEER

20/15
General Notes
All exposed corners to have $\frac{3}{4}$ " chamfer unless otherwise noted.
Precast concrete handrail to be 1:1:2 mix. Max aggregate is $\frac{5}{8}$ ".
All concrete, except handrail, to be Glass "S".
Reinforcing steel to be deformed bars of structural or intermediate grade.
Shop list and bending diagrams must be submitted by Contractor before fabrication is begun.
Roadway drains and expansion devices to be paid for at unit price bid for reinforcing steel.
Specifications: Arkansas Standard Road & Bridge Specifications, adopted May 30, 1925 and revised.
Unit Stresses: $f_s = 16,000 \text{ psi}$, $f_c = 750 \text{ psi}$, $n = 15$
Loading 1115
Gibbs 4-30-30 by Nee

DETAIL OF
STANDARD 30' R.C.D. GIRDER
4 GIRDER TYPE-4 PILE BENT
20 0 CLEAR ROADWAY

ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

Drawn By: ELK Date: 3-7-30
 Traced By: SR Date: 3-12-30
 Checked By: _____ Date: _____
 Scale: As noted.
 BRIDGE NO. _____ DRAWING NO. 2273